

TECHNICAL SCHEDULES

Schedule-A

(See Clauses 2.1 and 8.1)

Site of the Project

1. The Site

- (i) Site of the 2-lane project highway shall include land, buildings, structures and road works as described in **Annex-I** of this Schedule-A.
- (ii) The dates of handing over the Right of Way to the Contractor are specified in **Annex-II** of this Schedule-A.
- (iii) An inventory of the Site including the land, buildings, structures, road works, trees and any other immovable property on, or attached to, the Site shall be prepared jointly by the Authority Representative and the Contractor, and such inventory shall form part of the memorandum referred to in Clause 8.2 (i) of this Agreement.
- (iv) The alignment plans of the Project Highway are specified in **Annex-III**. The proposed profile of the Project Highways shall be followed by the contractor with minimum FRL as indicated in the alignment plan. The Contractor, however, improve/upgrade the Road Profile as indicated in Annex-III based on site/design requirement.
- (v) The status of the environment clearances obtained or awaited is given in **Annex-IV**.

Annex - I
(Schedule-A)
Site

1. Site

The site of the 2-lane project highway comprises section of National Highway-244 commencing from km 31+449 (Khellani at km 44+946) to km 51+700 (Premnagar at km 68+617) of length 20+251 km i.e. Khellani-Kishtwar-Chattroo_Khanabal section in the Union Territory of Jammu & Kashmir. The land, carriageway and structures comprising the Site are described below.

2. Land

The Site of the Project Highway comprises the land (sum of land already in possession and land to be possessed) as described below:

| Sr. No. | Chainage (km) | | Right of Way (m) | Remarks |
|---------|---------------|--------|-------------------|----------|
| | From | To | | |
| 1 | 31+500 | 31+900 | 7 | |
| 2 | 31+900 | 32+040 | 5 | |
| 3 | 32+040 | 35+280 | Realignment | Pul Doda |
| 4 | 35+280 | 36+200 | 6.5 | |
| 5 | 36+200 | 36+300 | Curve improvement | |
| 6 | 36+300 | 37+100 | 5 | |
| 7 | 37+100 | 37+235 | Curve improvement | |
| 8 | 37+235 | 37+600 | 5.5 | |
| 9 | 37+600 | 38+200 | 7 | |
| 10 | 38+200 | 38+500 | 8 | |
| 11 | 38+500 | 40+000 | 6 | |
| 12 | 40+000 | 40+400 | 8 | |
| 13 | 40+400 | 43+100 | 6 | |
| 14 | 43+100 | 43+700 | 7 | |
| 15 | 43+700 | 46+500 | 6 | |
| 16 | 46+500 | 47+600 | 7 | |
| 17 | 47+600 | 47+750 | Curve improvement | |
| 18 | 47+750 | 49+450 | 6 | |
| 19 | 49+450 | 50+300 | 7 | |
| 20 | 50+300 | 51+050 | 6 | |
| 21 | 51+050 | 51+700 | 7 | |

3. Carriageway

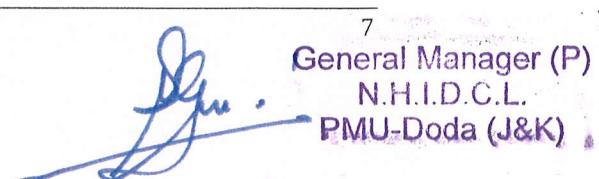
The existing carriageway is of 7 m width without paved shoulders.

4. Major Bridges

The Site includes the following Major Bridges:

| Sr. No. | Ex Chainage (km) | Type of Structure | | | No. of Spans with span length (m) | Overall Width (m) |
|---------|------------------|-------------------|---------------|-----------------|-----------------------------------|-------------------|
| | | Foundation | Sub-structure | Super-structure | | |
| 1 | 53+800 | Open | RCC | Steel Truss | 1X70 | 13.25 |

5. Road over-bridges (ROB) / Road under-bridges (RUB)


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The Site includes the following ROB (road over railway line)/RUB (road under railway line):

| Sr. No. | Chainage (km) | Type of Structure | | No. of Spans with span length (m) | Width (m) | ROB/ RUB |
|---------|---------------|-------------------|----------------|-----------------------------------|-----------|----------|
| | | Foundation | Superstructure | | | |
| | | | | Nil | | |

6. Grade separators

The Site includes the following grade separators:

| Sr. No. | Chainage (km) | Type of Structure | | No. of Spans with span length (m) | Width (m) |
|---------|---------------|-------------------|----------------|-----------------------------------|-----------|
| | | Foundation | Superstructure | | |
| | | | | Nil | |

7. Minor bridges

The Site includes the following minor bridges:

| Sr. No. | Ex Chainage (km) | Type of Structure | | | No. of Spans with span length (m) | Overall Width (m) |
|---------|------------------|-------------------|---------------|--------------------|-----------------------------------|-------------------|
| | | Foundation | Sub-structure | Super-structure | | |
| 1 | 63+050 | - | RCC | Solid Slab | 1X9.0 | 10.5 |
| 2 | 64+875 | Open | Masonry | Steel Plate Girder | 1x22.5 | 8.7 |
| 3 | 68+050 | Open | Masonry | Steel Plate Girder | 1x24.5 | 8.9 |

8. Railway level crossings

The Site includes the following railway level crossings:

| Sr. No. | Location (km) | Remarks |
|---------|---------------|---------|
| | | Nil |

9. Underpasses (vehicular, non-vehicular)

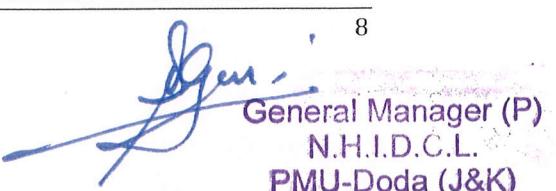
The Site includes the following underpasses:

| Sr. No. | Chainage (km) | Type of Structure | No. of Spans with span length (m) | Width (m) |
|---------|---------------|-------------------|-----------------------------------|-----------|
| | | | Nil | |

10. Culverts

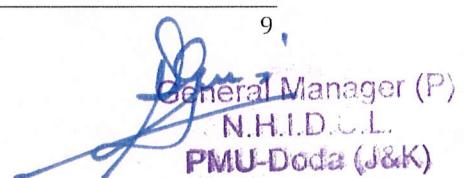
The Site has the following culverts:

| Sr.N o. | Existing chainage | Type of Structure | Span Arrangement | | Width in m |
|---------|-------------------|-------------------|------------------|----------------|------------|
| | | | No. | Clear Span (m) | |
| 1 | 44+900 | Slab | 1 | 1.60 | 10.50 |
| 2 | 45+100 | Slab | 1 | 2.00 | 10.30 |
| 3 | 45+500 | Slab | 1 | 2.00 | 9.80 |
| 4 | 45+800 | Slab | 1 | 1.20 | 10.00 |
| 5 | 45+900 | Slab | 1 | 2.00 | 10.90 |
| 6 | 46+000 | Slab | 1 | 1.00 | 9.60 |
| 7 | 46+300 | Slab | 1 | 2.00 | 10.30 |
| 8 | 46+800 | Slab | 1 | 2.00 | 13.00 |
| 9 | 47+100 | Slab | 1 | 2.00 | 10.20 |
| 10 | 47+200 | Slab | 1 | 2.00 | 10.20 |
| 11 | 47+500 | Slab | 1 | 2.00 | 10.30 |
| 12 | 47+800 | Slab | 1 | 3.00 | 10.30 |
| 13 | 47+850 | Slab | 1 | 2.00 | 10.30 |


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| Sr.N o. | Existing chainage | Type of Structure | Span Arrangement | | Width in m |
|------------|----------------------|----------------------|------------------|----------------|------------|
| | | | No. | Clear Span (m) | |
| 14 | 48+500 | Slab | 1 | 1.60 | 10.20 |
| 15 | 48+600 | Slab | 1 | 1.60 | 10.20 |
| 16 | 48+700 | Slab | 1 | 1.60 | 10.20 |
| 17 | 48+850 | Slab | 1 | Blocked | |
| 18 | 48+900 | Slab | 1 | 2.00 | 10.30 |
| 19 | 49+000 | Slab | 1 | 1.60 | 10.20 |
| 20 | 49+100 | Slab | 1 | 2.20 | 10.20 |
| 21 | 49+300 | Slab | 1 | 2.00 | 10.20 |
| 22 | 49+600 | Slab | 1 | 2.00 | 10.30 |
| 23 | 50+000 | Slab | 1 | 2.00 | 10.20 |
| 24 | 50+300 | Slab | 1 | 2.00 | 10.30 |
| 25 | 50+500 | Slab | 1 | 2.00 | 10.30 |
| 26 | 50+600 | Slab | 1 | 3.00 | 10.20 |
| 27 | 50+750 | Slab | 1 | 2.20 | 10.30 |
| 28 | 50+900 | Slab | 1 | 2.00 | 10.20 |
| 29 | 51+100 | Slab | 1 | 2.00 | 10.30 |
| 30 | 51+200 | Slab | 1 | 2.00 | 10.20 |
| 31 | 52+400 | Slab | 1 | Blocked | |
| 32 | 52+700 | Slab | 1 | 1.00 | 10.20 |
| 33 | 52+800 | Slab | 1 | 3.00 | 10.20 |
| 34 | 53+250 | Slab | 1 | 2.00 | 10.00 |
| 35 | 53+800 | Causeway | 1 | 4.00 | |
| 36 | 54+264 | Slab | 1 | 2.00 | 12.00 |
| 37 | 56+900 | Slab | 1 | 2.00 | 10.30 |
| 38 | 57+260 | Slab | 1 | 2.00 | 12.00 |
| 39 | 57+400 | Pipe | 2 | 0.90 | 10.20 |
| 40 | 57+750 | Slab | 1 | 2.00 | 10.20 |
| 41 | 58+270 | Slab | 1 | 2.00 | 10.30 |
| 42 | 58+405 | Pipe | 1 | 0.60 | 10.20 |
| 43 | 58+540 | Causeway | 1 | 4.00 | 10.20 |
| 44 | 58+850 | Slab | 1 | 3.00 | 10.30 |
| 45 | 59+080 | Slab | 1 | 2.00 | 10.20 |
| 46 | 59+250 | Slab | 1 | 3.00 | 10.20 |
| 47 | 59+570 | Slab | 1 | 3.00 | 10.20 |
| 48 | 60+100 | Slab | 1 | 1.00 | 10.30 |
| 49 | 60+370 | Pipe | 1 | 0.60 | 10.20 |
| 50 | 60+675 | Slab | 1 | 2.00 | 10.30 |
| 51 | 61+170 | Slab | 1 | 3.00 | 10.20 |
| 52 | 61+510 | Slab | 1 | 2.00 | 10.20 |
| 53 | 61+785 | Pipe | 1 | 0.60 | 10.20 |
| 54 | 62+175 | Slab | 1 | 2.00 | 10.30 |
| 55 | 62+520 | Slab | 1 | 2.00 | 10.30 |
| 56 | 62+770 | Pipe/Slab | 1 | 0.6/1.6 | 10.30 |
| 57 | 63+265 | Slab | 1 | 2.00 | 10.20 |
| 58 | 63+590 | Pipe | 1 | 0.60 | 10.20 |
| 59 | 63+660 | Slab | 1 | 1.50 | 10.20 |
| 60 | 63+755 | Slab | 1 | 1.50 | 10.30 |
| 61 | 63+945 | Slab | 1 | 2.00 | 10.20 |
| 62 | 65+415 | Slab | 1 | 1.50 | 10.30 |
| 63 | 65+500 | Slab | 1 | 2.80 | 10.20 |
| 64 | 65+775 | Slab | 1 | 2.80 | 10.20 |


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| Sr.N o. | Existing chainage | Type of Structure | Span Arrangement | | Width in m |
|------------|----------------------|----------------------|------------------|----------------|------------|
| | | | No. | Clear Span (m) | |
| 65 | 66+050 | Slab | 1 | 2.80 | 10.20 |
| 66 | 66+700 | Slab | 1 | 2.80 | 10.20 |

11. Bus bays

The details of bus bays on the Site are as follows:

| Sr. No. | Chainage (km) | Length (m) | Left Hand Side | Right Hand Side |
|---------|---------------|------------|----------------|-----------------|
| | | Nil | | |

12. Truck Lay byes

The details of truck lay byes are as follows:

| Sr. No. | Chainage (km) | Length (m) | Left Hand Side | Right Hand Side |
|---------|---------------|------------|----------------|-----------------|
| | | Nil | | |

13. Roadside drains

The details of the roadside drains are as follows:

| Sr. No. | Location | | Type | |
|---------|----------|-------|--------------------|------------------|
| | From km | to km | Masonry/cc (Pucca) | Earthen (Kutcha) |
| | | Nil | | |

14. Major junctions

The details of major junctions are as follows:

| Sr. No. | Location | | At grade | Separated | Category of Cross Road | | | |
|---------|----------|-------|----------|-----------|------------------------|----|-----|--------|
| | From km | to km | | | NH | SH | MDR | Others |
| | | Nil | | | | | | |

(NH: National Highway, SH: State Highway, MDR: Major District Road)

15. Minor junctions

The details of the minor junctions are as follows:

| Sr. No. | Existing Chainage | Remarks | Side | Type |
|---------|-------------------|----------------|------|------|
| 1 | 46+910 | Minor Junction | RHS | Y |
| 2 | 48+150 | Minor Junction | RHS | Y |
| 3 | 53+082 | Minor Junction | LHS | Y |
| 4 | 53+950 | Major Junction | RHS | Y |
| 5 | 54+210 | Minor Junction | LHS | Y |
| 6 | 57+050 | Minor Junction | RHS | Y |
| 7 | 60+680 | Minor Junction | RHS | Y |
| 8 | 61+750 | Minor Junction | RHS | Y |

16. Bypasses

The details of the existing road sections proposed to be bypassed are as follows:

| Sr. No. | Name of bypass (town) | Chainage (km) From km to km | Length (in Km) |
|---------|-----------------------|-----------------------------|----------------|
| | | Nil | |

17. Others

Nil

(As per Clause 8.3 (i))

(Schedule-A)

Dates for providing Right of Way of Construction Zone

The dates on which the Authority shall provide Right of Way of Construction Zone to the Contractor on different stretches of the Site are stated below:

| Sl. No. | From (Km) | To (Km) | Length (Km) | Width (m) | Date of providing |
|------------------------------------|-----------|---------|-------------|-----------|---|
| | | | | | Right of Way* |
| 1 | 2 | 3 | 4 | 5 | |
| (i) Full Right of Way (Full Width) | 31+449 | 33+335 | 1886 | 21 | 150 (one hundred and fifty) days after the Appointed Date |
| | 33+335 | 33+840 | 505 | 24 | |
| | 33+840 | 34+675 | 835 | 29 | |
| | 34+675 | 34+940 | 265 | 30 | |
| | 34+940 | 35+340 | 400 | 24 | |
| | 35+340 | 35+650 | 310 | 22 | |
| | 35+650 | 36+270 | 620 | 24 | |
| | 36+270 | 37+100 | 830 | 22 | |
| | 37+100 | 37+235 | 135 | 16 | |
| | 37+235 | 37+900 | 665 | 24 | |
| | 37+900 | 44+755 | 6855 | 22 | |
| | 44+755 | 44+825 | 70 | 20 | |
| | 44+825 | 45+000 | 175 | 18 | |
| | 45+000 | 47+550 | 2550 | 22 | |
| | 47+550 | 47+880 | 330 | 20 | |
| | 47+880 | 50+950 | 3070 | 23 | |
| | 50+950 | 51+160 | 210 | 20 | |
| | 51+160 | 51+700 | 540 | 23 | |
| (ii) Part Right of way | 31+500 | 31+900 | 400 | 7 | On the appointed date |
| | 31+900 | 32+040 | 140 | 5 | |
| | 35+280 | 36+200 | 920 | 6.5 | |
| | 36+300 | 37+100 | 800 | 5 | |
| | 37+235 | 37+600 | 365 | 5.5 | |
| | 37+600 | 38+200 | 600 | 7 | |
| | 38+200 | 38+500 | 300 | 8 | |
| | 38+500 | 40+000 | 1500 | 6 | |
| | 40+000 | 40+400 | 400 | 8 | |
| | 40+400 | 43+100 | 2700 | 6 | |
| | 43+100 | 43+700 | 600 | 7 | |
| | 43+700 | 46+500 | 2800 | 6 | |
| | 46+500 | 47+600 | 1100 | 7 | |
| | 47+750 | 49+450 | 1700 | 6 | |

Construction & Up gradation to 2 lane with paved shoulder from Design Km. 31.449 to Km 51.700 of Khellani-Khanabal Section on NH-244 (Pkg-I)

| Sl. No. | From (Km) | To (Km) | Length (Km) | Width (m) | Date of providing |
|----------|-----------|----------|-------------|-----------|-------------------|
| | | | | | Right of Way* |
| 1 | | 2 | 3 | 4 | 5 |
| | 49+450 | 50+300 | 850 | 7 | |
| | 50+300 | 51+050 | 750 | 6 | |
| | 51+050 | 51+700 | 650 | 7 | |

*The dates specified herein shall in no case be beyond 150 (one hundred and fifty) days after the Appointed Date.

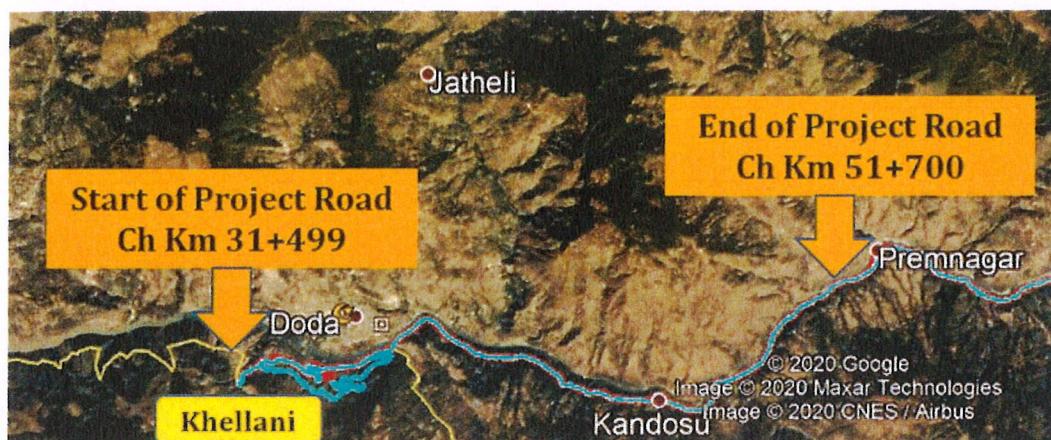
Annex - III

(Schedule-A)

Alignment Plans

The alignment of the Project Highway shall be modified in the following sections as per the alignment plan indicated below:

(i) The alignment of the Project Highway is enclosed in alignment plan and indicated below. Finished road level indicated in the alignment plan shall be followed by the contractor as minimum FRL. In any case, the finished road level of the project highway shall not be less than those indicated in the alignment plan. The contractor shall, however, improve/upgrade the Road profile as indicated in Annex-III based on site/design requirement.



Annex - IV
(Schedule-A)
Environment Clearances

As per EIA notification 2006 and its amendment S.O.2559 (E) Dt 22nd August 2013, S.O 996(E) Dt 10th April 2015, S.O 382(E) Dt 3rd February 2015 Environmental Clearance Exempted from the purview of the Environmental Impact Assessment .

[To be published in the Gazette of India, Extraordinary, Part II, Section 3, Sub-section(ii)]

**MINISTRY OF ENVIRONMENT AND FORESTS
NOTIFICATION**

New Delhi, the 22nd August, 2013

S.O. 2559 (E).- Whereas by notification of the Government of India in the Ministry of Environment and Forests vide number S.O.1533(E), dated the 14th September, 2006 issued under sub-section (1) and clause (v) of sub-section (2) of section (3) of the Environment (Protection) Act, 1986 read with clause (d) of sub-rule (3) of rule 5 of the Environment (Protection) Rules, 1986, the Central Government directed that on and from the date of its publication, the required construction of new projects or activities or the expansion or modernization of existing projects or activities listed in the Schedule to the said notification entailing the capacity addition with change in process or technology and or product mix shall be undertaken in any part of India only after prior environmental clearance from the Central Government or as the case may be, by the State level Environment Impact Assessment Authority, duly constituted by the Central Government under sub-section (3) of section 3 of the said Act, in accordance with the procedure specified therein;

And whereas the Government of India in the Ministry of Environment and Forests had constituted a High Level Committee under the Chairmanship of Member (Environment and Forests and Science and Technology), Planning Commission, vide OM No.21-270/2008-IA.III dated the 11th December, 2012 to review the provisions of Environmental Impact Assessment Notification, 2006 relating to granting Environmental Clearances for Roads, Buildings and Special Economic Zone projects and provisions under the OM dated the 7th February, 2012 issued by the Ministry of Environment and Forests regarding guidelines for High Rise Buildings;

And whereas one of the terms of reference (ToR) of the Committee was to review the requirement of Environmental Clearance for highway expansion projects upto the right of way of 60 meters and length of 200 kms under Environmental Impact Assessment notification;

And whereas the Committee has submitted its report to the Ministry and on this ToR, the Committee has recommended exempting highway expansion projects from the requirement of scoping and that Environmental Impact Assessment or Environment Management Plan for highway expansion projects may be prepared on the basis of model ToRs to be posted on Ministry's website and in respect of requirement of environmental clearance, the Committee has recommended that expansion of National Highway projects up to 100 kms involving additional right of way or land acquisition upto 40 mts on existing alignments and 60 mts on re-alignments or by-passes may be exempted from the preview of the notification;

Schedule - B

(See Clause 2.1)

Development of the Project Highway

1. Development of the Project Highway

Development of the Project Highway shall include design and construction of the Project Highway as described in this Schedule-B and in Schedule-C.

2. Rehabilitation and augmentation

Nil

3. Specifications and Standards

The Project Highway shall be designed and constructed in conformity with the Specifications and Standards specified in Annex-I of Schedule-D.

Annex - I

(Schedule-B)

Description of the Project

Construction & up-gradation to 2 Lane with paved shoulder from Km 31+449 (Existing km 44+946) to Km 51+700 (Existing km 68+617) of length 20.251 Km on Khellani - Kishtwar - Chattroo - Khanabal section of NH-244 in the Union Territory of Jammu and Kashmiron EPC Mode.

1. Widening of Existing Highway

i. The Project Highway shall follow the existing alignment unless otherwise specified by the Authority and shown in the alignment plans specified in Annex-III of Schedule-A. Geometric deficiencies, if any, in the existing horizontal and vertical profiles shall be corrected as per the prescribed standards for hill/mountainous terrain to the extent landis available.

| SL. No. | Design Chainage (km) | | Length (km) | Remarks |
|---------|----------------------|--------|-------------|-------------------------------|
| | From | To | | |
| 1 | 31+449 | 31+900 | 0.451 | Widening with 2- lane with PS |
| 2 | 31+900 | 35+350 | 3.450 | New 2-lane with PS |
| 3 | 35+350 | 51+700 | 16.350 | Widening with 2- lane with PS |

ii. Width of Carriageway

(a) 2-Laningwith paved shoulders shall be undertaken for main road. The paved carriageway shall be 10m wideaccordance with the typical cross section's drawings attached along with Schedule B.

(b) Except as otherwise provided in this agreement, the width of the paved carriageway and cross-sectional features shall confirm to paragraph 1.1 above.

2. Geometric Design and General Features

(i) General

Geometric design and general features of the Project Highway shall be in accordance with Section 2 of the Manual.

(ii) Design speed

The design speed shall be the maximum design speed of 60 Km/hr. and minimum design speed of 40 km/hr. for mountainous/hilly terrain as perIRC: SP-73:2018 and IRC: SP-48:1998

(iii) Improvement of the existing road geometrics

In the following sections, where improvement of the existing road geometrics to the prescribed standards.

| Sl. No. | Stretch (from km to km) | Type of deficiency | Remarks |
|---------|-------------------------|--------------------|---------|
| | | Nil | |

(iv) Right of Way

Details of the Right of Way are given in Annex II of Schedule-A.

(v) Type of shoulders

(a) In built-up sections, footpaths/fully paved shoulders shall be provided in the following stretches:

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| Sl.No. | Stretch(from km to km) | Fully paved shoulders/ footpaths | Reference to cross section |
|--------|------------------------|----------------------------------|----------------------------|
| | | Nil | |

- (b) In open country/hilly areas, paved shoulders of 1.5m width shall be provided on either side and balance 1.0m width earthen shoulder at valley side only shall be covered with 150 mm thick compacted layer of granular material for main road.
- (c) Design and specifications of paved shoulders and granular material shall conform to the requirements specified in the relevant Manual.

(vi) Lateral and vertical clearances at underpasses

- (a) Lateral and vertical clearances at underpasses and provision of guardrails/crash barriers shall be as per the provision of relevant Manual.
- (b) Lateral clearance: The width of the opening at the underpasses shall be as follows:

| Sl.No. | Location (Chainage) (from km to km) | Span/ opening (m) | Remarks |
|--------|-------------------------------------|-------------------|---------|
| | | NIL | |

(vii) Lateral and vertical clearances at overpasses

- (a) Lateral and vertical clearances at overpasses shall be as per the provision of relevant Manual.
- (b) Lateral clearance: The width of the opening at the overpasses shall be as follows:

| Sl.No. | Location (Chainage) (from km to km) | Span/ opening (m) | Remarks |
|--------|-------------------------------------|-------------------|---------|
| | | Nil | |

(viii) Service roads

Service roads shall be constructed at the locations and for the lengths indicated below:

| Sl.No. | Location of service road (from km to km) | Right hand side (RHS)/Left hand side (LHS)/ or Both sides | Length (m) of service road |
|--------|--|---|----------------------------|
| | | | NIL |

(ix) Grade separated structures

- (a) Grade separated structures shall be provided as per provision of the relevant Manual. The requisite particulars are given below:

| Sl. No. | Location of structure | Length (m) | Number and length of spans (m) | Approach gradient | Remarks, if any |
|---------|-----------------------|------------|--------------------------------|-------------------|-----------------|
| | | | Nil | | |

In the case of grade separated structures, the type of structure and the level of the Project Highway and the crossroads shall be as follows:

| Sl. No. | Location | Type of structure Length (m) | Cross road at | | | Remarks, if any |
|---------|----------|------------------------------|----------------|--------------|---------------|-----------------|
| | | | Existing Level | Raised Level | Lowered Level | |
| | | | Nil | | | |

(x) Cattle and pedestrian underpass/overpass

Cattle and pedestrian underpass/ overpass shall be constructed as follows:

| Sl. No. | Location | Type of crossing |
|---------|----------|------------------|
| | | |

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PMU-Doda (J&K)

Nil

(xi) Typical cross-sections of the Project Highway

Following typical cross sections shall be provided for the Project Highway However to be designed as per manual.

| Sr. No. | Design Chainage | | Design Length | TCS Detail | TCS Type |
|---------|-----------------|--------|---------------|--|--------------|
| | From | To | | | |
| 1 | 31+449 | 31+493 | 43.5 | Two Lane C/W With PS With one side cut & one Side Fill & Protection as Applicable (Reconstruction) | TCS-3 |
| 2 | 31+493 | 31+508 | 15 | Viaduct | Viaduct |
| 3 | 31+508 | 31+560 | 52.5 | Two Lane C/W With PS with Both Side Fill & Protection as Applicable (Reconstruction) | TCS-1A |
| 4 | 31+560 | 31+650 | 90 | Two Lane C/W With PS With one side cut & one Side Fill & Protection as Applicable (Reconstruction) | TCS-3 |
| 5 | 31+650 | 31+810 | 160 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (Reconstruction) | TCS-1A |
| 6 | 31+810 | 31+900 | 90 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (Reconstruction) | TCS-3 |
| 7 | 31+900 | 31+970 | 70 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 8 | 31+970 | 32+070 | 100 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (New Construction) | TCS-1 |
| 9 | 32+070 | 32+180 | 110 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 10 | 32+180 | 32+230 | 50 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (New Construction) | TCS-1 |
| 11 | 32+230 | 32+350 | 120 | Two Lane C/W With PS With Both Side Cut & Protection As Applicable (New Construction) | TCS-4 |
| 12 | 32+350 | 32+390 | 40 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 13 | 32+390 | 32+420 | 30 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (New Construction) | TCS-1 |
| 14 | 32+420 | 32+470 | 50 | Minor Bridge | Minor Bridge |
| 15 | 32+470 | 32+560 | 90 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 16 | 32+560 | 32+718 | 157.5 | Two Lane C/W With PS With Both Side Cut & Protection As Applicable (New Construction) | TCS-4 |
| 17 | 32+718 | 32+733 | 15 | Minor Bridge | Minor Bridge |
| 18 | 32+733 | 32+780 | 47.5 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (New Construction) | TCS-1 |
| 19 | 32+780 | 33+440 | 660 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |

Construction & Up gradation to 2 lane with paved shoulder from Design Km. 31.449 to Km 51.700 of Khellani-Khanabal Section on NH-244 (Pkg-I)

| Sr. No. | Design Chainage | | Design Length | TCS Detail | TCS Type |
|---------|-----------------|--------|---------------|--|--------------------|
| | From | To | | | |
| 20 | 33+440 | 33+700 | 260 | Two Lane C/W With PS With Both Side Cut & Protection As Applicable (New Construction) | TCS-4 |
| 21 | 33+700 | 33+850 | 150 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 22 | 33+850 | 34+100 | 250 | Two Lane C/W With PS With Both Side Cut & Protection As Applicable (New Construction) | TCS-4 |
| 23 | 34+100 | 34+160 | 60 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (New Construction) | TCS-1 |
| 24 | 34+160 | 34+198 | 37.5 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 25 | 34+198 | 34+273 | 75 | Bridge Cum Viaduct | Bridge Cum Viaduct |
| 26 | 34+273 | 34+340 | 67.5 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 27 | 34+340 | 34+950 | 610 | Two Lane C/W With PS With Both Side Cut & Protection As Applicable (New Construction) | TCS-4 |
| 28 | 34+950 | 35+050 | 100 | Viaduct | Viaduct |
| 29 | 35+050 | 35+308 | 257.5 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 30 | 35+308 | 35+323 | 15 | Minor Bridge | Minor Bridge |
| 31 | 35+323 | 35+360 | 37.5 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (New Construction) | TCS-2 |
| 32 | 35+360 | 35+480 | 120 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (Reconstruction) | TCS-3 |
| 33 | 35+480 | 35+530 | 50 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (Reconstruction) | TCS-1A |
| 34 | 35+530 | 35+570 | 40 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (Reconstruction) | TCS-3 |
| 35 | 35+570 | 35+630 | 60 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (Reconstruction) | TCS-1A |
| 36 | 35+630 | 36+200 | 570 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (Reconstruction) | TCS-3 |
| 37 | 36+200 | 36+240 | 40 | Two Lane C/W With PS With Both Side Fill & Protection As Applicable (Reconstruction) | TCS-1A |
| 38 | 36+240 | 36+260 | 20 | Minor Bridge | Minor Bridge |
| 39 | 36+260 | 37+112 | 852 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (Reconstruction) | TCS-3 |
| 40 | 37+112 | 37+202 | 90 | Major Bridge | Major Bridge |
| 41 | 37+202 | 41+280 | 4078 | Two Lane C/W With PS With one side cut & one Side Fill & Protection As Applicable (Reconstruction) | TCS-3 |